

RETAINING WALL CONSTRUCTION PERMIT APPLICATION

CITY OF UNDERWOOD, IOWA Building & Safety Department

241 Third Street P.O. Box 40 Underwood, IA 51576 Telephone: (712) 566-2373 Fax: (712) 566-2083 Inspection Request: (712) 309-2935

		Permit Amount	Town	ship	Permit Number
JOB SITE ADDRESS:			PARCEL N	UMBER:	
LEGAL DESCRIPTION: Attachment					ACREAGE:
ZONING DISTRICT : 🗌 OS-A Open Space- Agriculture	e 🔲 R-1 Single Famil	y Residential Distri	ct 🗌 R-2 1	「wo Family R	esidential District
R-3 Multiple Family Residential District	R-1M Single Fan	nily Mobile/Manufac	tured Home	Residential D	District
DC Downtown Commercial District	CC Corridor Cor	nmercial District	🗌 M-1	Light Industr	ial District
M-2 General Industrial District	🗌 FP Flood Plain D	istrict	🗌 FW	Flood Way D	istrict
PROPERTY OWNER:				PHONE NU	MBER:
PROPERTY OWNERS ADDRESS:				STATE:	ZIP CODE:
GENERAL CONTRACTOR NAME:		STATE	LICENSE #:	PHONE NU	MBER:
CONTRACTOR MAILING ADDRESS:				STATE:	ZIP CODE:
Property Type/Use: Commercial / Industrial R	esidential 🗌 Multi-	Family D Other			
Class of Work: New Addition	Iteration Repa	ir 🗌 Replac	ement		

The wall/fence as described below is totally within the boundaries of the property.
Yes No

The wall/fence as described below is located on the property line.

See the attached notarized authorization letter from the adjacent property owner. Yes No (If Required)

CONSTRUCTION INFORMATION

🗌 Yes 🗌 No

PROPOSED CONSTRUCTION DESCRIPTION:

	TRUCTION COST:	
PERMIT FEES		AMOUNTS
After calculating the square footage with the permit valuation multiplier and determining your total permitted valuation. Use the Building Permit Fee Schedule and determine your Building Permit Fee. This permit fee will be verified during plan review and collected at the time of permit issuance. (see next page to figure cost)	Building Permit Fee Receipt #:	\$
Figuring the Plan Review Fee at 25% of the calculated Building Permit Fee cost. The Plan Review Fee will be a required deposit at the time of your permit application submittal. (see next page to figure cost)	Plan Review Fee (submittal deposit) Receipt #:	\$
	Total Amount	\$

I have indicated all natural and man-made water courses which may have impact on or be impacted by the proposed retaining wall, block wall or fence. I understand and agree that should the City of Underwood determine that this retaining wall, block wall or fence be detrimental to the safe flow of any water course, this permit will be rendered invalid immediately. I further agree that if I fail to adhere to the above requirements, the retaining wall, block wall or fence may be abated, removed or altered at my expense. I certify that I have read this Application and state that the above information is correct. I agree to comply with the City ordinances and state laws relating to building construction.

APPLICANTS SIGNATURE	DATE

Issued By:

Date:

BUI	LDING PERMIT FEE SC	HEDULE
TOTAL VALUATION	FEE	
\$1 to \$500	\$ 23.50	
\$501 to \$2,000	\$ 23.50 for the first \$ 500.00 plus \$ 100.00, or fraction thereof, to a	
\$2,001 to \$25,000	\$ 69.25 for the first \$ 2,000.00 pl \$ 1,000.00, or fraction thereof, to	
F	PLANS ATTACHED NO F	
Lineal Feet	@ 6' and Higher x \$4.75 =	Permit Valuation
Lineal Foot	@ 4' and Higher x \$3.50 =	Permit Valuation
Note: Wall measured	in overall height from bottom of t	ooting to finished top of wall.
		Building Fee:
		Bullang i cc.
		Bldg Plan Review Fee:
		Bldg Plan Review Fee: Total Fee:
		-
	EXAMPLE ONL	Total Fee:
Total Line	EXAMPLE ONL eal Feet. @ 4' to 6' High x \$3.50 = _	Total Fee:
Total Line		Total Fee:
	eal Feet. @ 4' to 6' High x \$3.50 = _	Total Fee: <u>Y</u> <u>\$875.00</u> Permit Valuation <u>e</u>
<u>250</u> Total Line For the first \$500.00 =	eal Feet. @ 4' to 6' High x \$3.50 = _ Lineal Feet to Square Footag	Total Fee: <u>**</u> <u>\$875.00</u> Permit Valuation <u>Permit Valuation</u> <u>Permit Valuation</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u> <u>**</u>
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<u>250</u> Total Line For the first \$500.00 = (\$875.00 – \$500.00 =	eal Feet. @ 4' to 6' High x \$3.50 =	Total Fee: .★

Total Barrier Permit Fee <u>\$ 62.75</u>

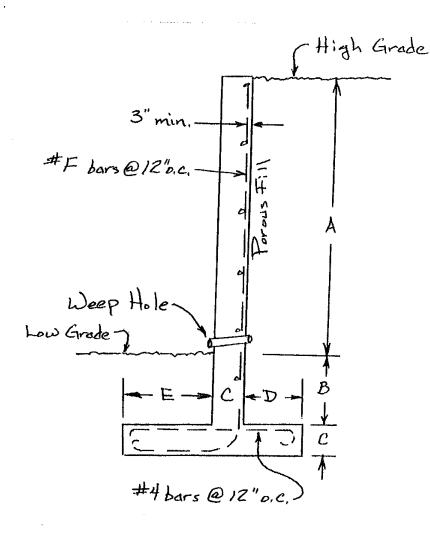
EXAMPLE ONLY

Please make be certain that you want to proceed with this project when you submit your application. The fees that you submit are not refundable once the application is submitted.

REINFORCED CONCRETE RETAINING WALLS CANTILEVER TYPE

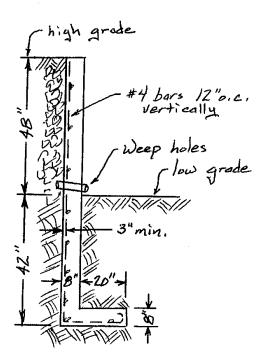
A	В	С	D	E	T F
10'	30"	12"	26"	38"	#8
8'	31"	11"	21"	34"	#6
6'	33"	9"	18"	25"	#6
4'	34"	8"	12"	14"	#4

Weep Holes to be two inch p.v.c. at ten feet on center.



REINFORCED CONCRETE RETAINING WALLS

Designs are based on adequate soil to resist to pressure and an angle of repose of thirty-three degrees, which is for average soil. Horizontal bars to prevent cracking are to be #3 rods, at two feet on center, with construction joints thirty feet apart. If construction joints are omitted, heavier bars must be used. No surcharge has been figured in the design of these walls.





Plan View Construction Joint